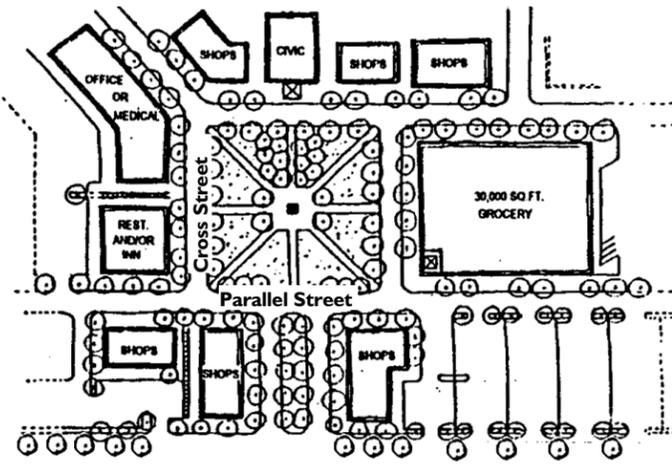
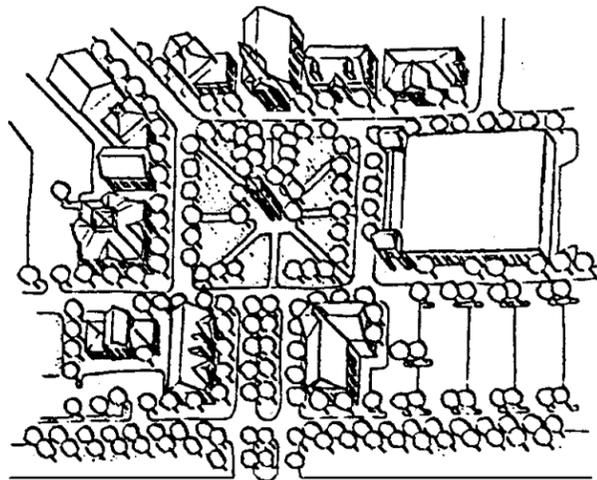


Commercial Mixed-Use Center

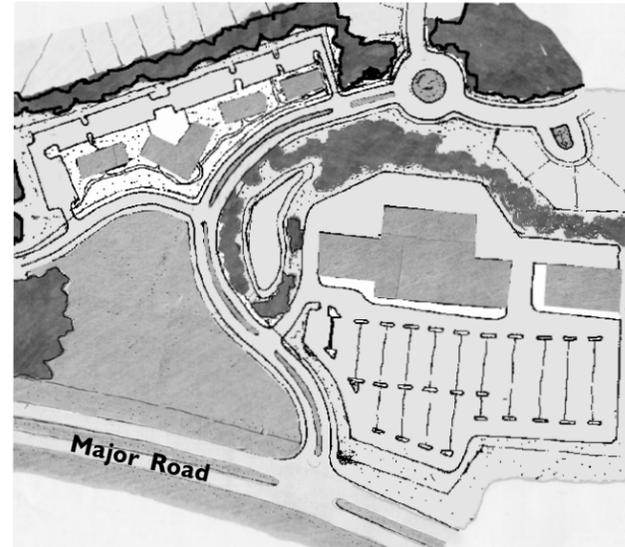


Major Road

- Exterior public spaces should be designed to enhance surrounding buildings and serve as an amenity for users.
- Buildings shall respect the context in which they are located by generally relating in scale and design features to other surrounding buildings and supporting uses (with the exception of pre-existing buildings inconsistent with these commercial design principles).
- Areas designed as conventional parking lots shall be located to the side and rear of buildings and should not be located in front of buildings.
- Civic uses are encouraged to be located as focal points or around public spaces, such as the town green, square, or park.

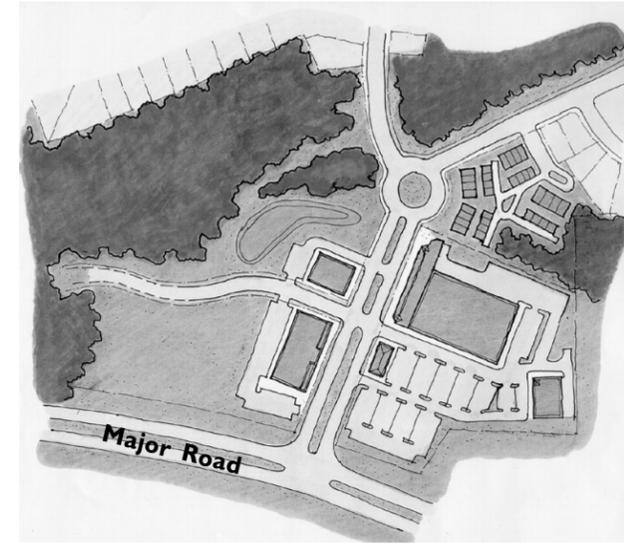


Alternative Commercial Mixed-Use Site Plans



Original "Strip" Shopping Center

- Parking lots located in front of buildings along the street create an auto-oriented environment that is hostile to pedestrians.
- They also provide poor visual quality and distance visibility to display windows for motorists.



Redesigned "Main Street" Shopping Center

- The buildings frame the entry street and create a more interesting and pedestrian-friendly streetscape.
- A more user-friendly layout provides better visibility for commercial and retail establishments for both pedestrians and motorists.
- The public greenspace in the roundabout terminates a well-defined axis.

LAND USE & COMMUNITY DESIGN: Commercial/Mixed-Use Design Principles

The Alternative Plan concentrates future development in appropriately sized, scaled and designed mixed-use Commercial Centers. The Plan is attempting to minimize strip commercial development which is not appropriate for Gray's Creek. A variety of uses is encouraged in the Centers. Office and retail should be the primary land uses; but higher-density residential, public and quasi-public uses are also encouraged. These latter uses add a civic flavor to commercial areas and can become gathering spots for the community. Higher-density residential also adds life beyond the normal "9 to 5" business hours and can help support uses such as cafes, movie theaters, and cultural venues.

The hypothetical site plans at left represent one manner in which mixed-use Commercial Centers could be designed. Obviously, the size of the Center will impact the layout and design elements. The site plans on this page illustrate the hypothetical design for a mixed-use Community Center. The design principles outlined in the Alternative Plan and contained on sheet 15 should be incorporated into future commercial development in mixed-use Neighborhood and Regional Centers.

Gray's Creek Area Plan

Memphis & Shelby County Office of Planning & Development

Memphis, Tennessee

05.27.99

Sheet 15 of 24

© 1999 Looney Ricks Kiss Memphis Nashville Princeton



Through its lack of defined, usable open space, conventional urban sprawl development fails to preserve valuable natural resources and features.

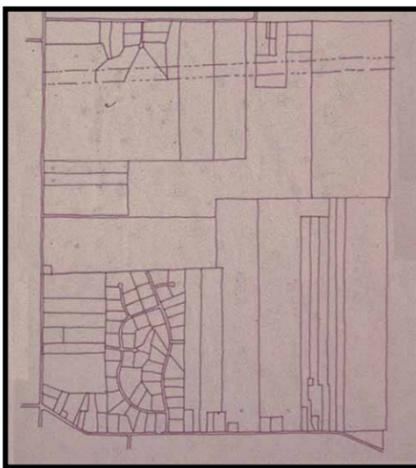


In contrast, cluster development allows the flexibility of fitting housing units around the existing natural landscape, as well as the preservation of usable open space.

Photo courtesy of Randall Arendt

Alternative Development Patterns

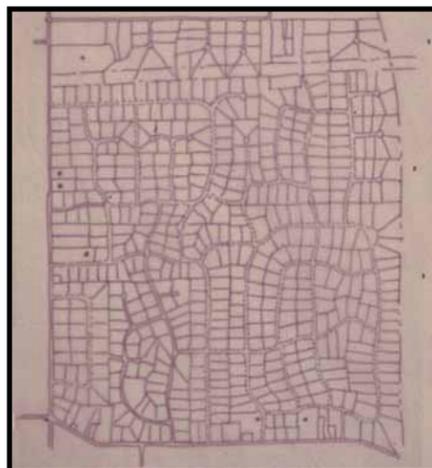
The following sketches reflect alternative development patterns for a portion of the Gray's Creek Area located in the easterly half of the study area and bound by George R. James Road to the north, Macon Road to the south, Fayette County to the east, and Collierville-Arlington Road to the west. The area is approximately 1,425 acres.



Existing Condition:

Approximately 121 Lots

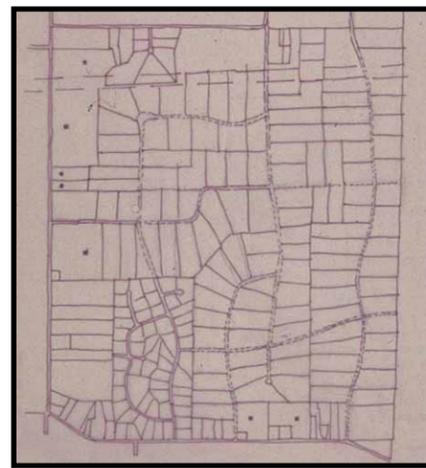
The existing condition of this area consists of large parcels having a rural density and newer conventional development featuring cul-de-sacs and 2-acre lots.



2-Acre Lots:

Approximately 585 Lots

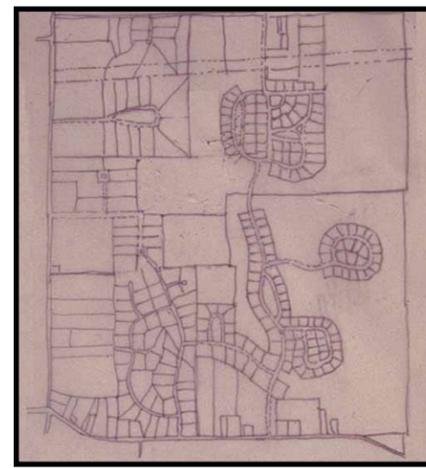
This scenario features the balance of the area being built-out at approximately the same 2-acre density subdivision. Although the hypothetical future development has a road system providing for greater connectivity than the existing development, no common open space is provided, and the character is very urban sprawl.



4-Acre Lots:

Approximately 237 Lots

This scenario has some similarities to the 2-acre scenario but at a lower density of 4-acre lots. While a well-connected road system is provided, all land is privatized on lots, providing no common open space.



Cluster Option:

Approximately 324 Lots

This scenario illustrates a variety of conditions, including the existing conventional development and several large rural parcels along Collierville-Arlington Road. However, the easterly half of the area features a cluster option in which approximately 1-acre lots are concentrated and surrounded by common open space. A variant of the cluster is illustrated in the northwest part of the area, in which 3- and 4-acre lots are clustered around a rural loop road, with open space preserved within the loop and along the frontage of Collierville-Arlington Road.

CREATIVE ZONING ALTERNATIVES & OTHER ISSUES

Potential alternatives to standard zoning approaches for the Gray's Creek Area may include clustered development, corridor overlay zones (see sheet 21), and special character zones (see sheet 23). While those techniques will require further exploration, they may ultimately be very useful for the Gray's Creek Area.

Cluster Development

In contrast to conventional subdivisions, cluster development is intended to preserve natural features, provide for more open space and reduce the amount of impervious cover by allowing for smaller networks of streets and utilities. Cluster development has the potential to be an especially important option in the aquifer recharge zone. In order to encourage the clustering option, developers would receive density bonuses and be given the option of building more units than is typically allowed under the existing zoning. (See Table 4, pg. 15, in the *Gray's Creek Area Background Study* for details regarding the bonus program.) It is recommended that the cluster option not be applied to areas where the base zoning allows for lots of less than 2 acres in size, as the resulting clustered lots would otherwise be less than 14,000 sq. ft., which would not be accepted by the area's real estate market. A developer must also provide 50% open space protected by a conservation easement in perpetuity to qualify for the density bonus. An on-site treatment system is one option for servicing cluster developments. While this creative zoning alternative may have great potential for the Gray's Creek Area, it is not among this Plan's formal recommendations until the idea can be explored in greater detail.

Other Issues

Efforts should be made to protect the natural vegetation, especially large specimen trees, in the study area. Permits should be required for any grading, and such grading should not be speculative and excessive but must be consistent with approved development plans. The formation of a land trust should be explored as a means of preserving open space in the study area. The recommendations in the "Metro Stormwater Study" should be followed to control stormwater runoff in the study area. No billboards should be allowed in the study area.

Gray's Creek Area Plan

Memphis & Shelby County Office of Planning & Development

Memphis, Tennessee

05.27.99

Sheet 16 of 24

© 1999 Looney Ricks Kiss Memphis Nashville Princeton

GREENWAYS & AQUIFER RECHARGE AREA

Greenway

Floodplains of Gray's Creek, Mary's Creek, and the Wolf River should be utilized to form a system of open spaces and greenways to serve the future residential population. Stream corridors and their associated floodplains are an ideal location for greenways, because floodplains:

- will not contain significant development
- are linear in form along streams and provide for connectivity
- constitute environmentally sensitive lands most in need of protection
- are the least expensive lands because of their limited development value

It is recommended that a 26-mile greenway system be developed to include Gray's Creek, Mary's Creek and the Wolf River. Also, when acceptable to utility operators, utility easements should be used for greenways, which could add an additional 14 miles. An asphalt path a minimum of 10 feet in width should be developed to accommodate walkers, joggers, skaters, and cyclists. For more information, see the current greenway study.

Aquifer

Since the inception of the planning process for Gray's Creek, the location of the boundaries for the aquifer recharge zone has been called into question. It is recommended that a comprehensive study be conducted to more accurately delineate the aquifer recharge area. If a site is determined to be in the aquifer recharge area and sewers are not available, densities should not exceed 1 unit per 2 acres. It also should be noted that wetlands trigger this low-density designation. A map delineating wetlands in the study area is attached to the *Gray's Creek Area Background Study*.

Gray's Creek Area Plan

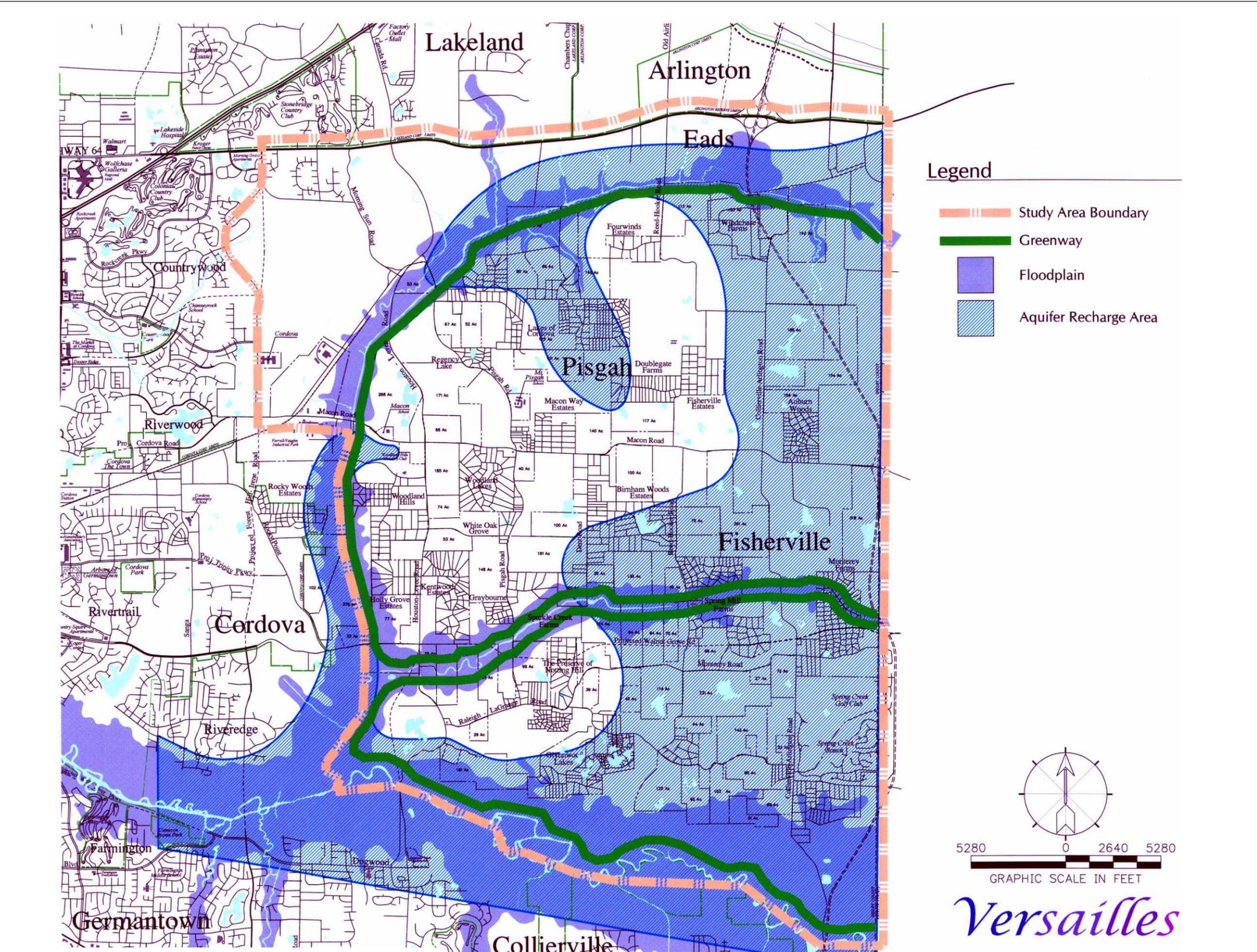
Memphis & Shelby County Office of Planning & Development

Memphis, Tennessee

05.27.99 Rev - Summer 2001

Sheet 17 of 24

© 1999 Looney Ricks Kiss Memphis Nashville Princeton



"Some experts have suggested that the deep aquifer known as the Memphis Sand is more vulnerable than once believed to the seepage of pollutants...it would be foolhardy for local and state officials throughout the Mid-South not to consider long-range threats both to the adequacy of supply and to the quality of available groundwater sources."
 Editorial, *The Commercial Appeal*, November 20, 1998.

* Delineation of the aquifer recharge area id preliminary until proper testing can occur.

MAJOR ROAD PLAN

The map on this page illustrates the Major Road Plan, which is an official document of the Memphis Planning Organization (MPO). The MPO is responsible for coordinating transportation planning for all of Shelby County, the northern portion of Desoto County, and the western portion of Fayette County. The Major Road Plan will have to be amended in order to implement many of the recommendations included on the Circulation Plan (sheet 19). Subsequent to the adoption of this *Gray's Creek Area Plan* by the County Commission, the circulation element should be presented to the MPO as an amendment to the existing Major Road Plan.

Projects included on the Major Road Plan are organized according to construction priority. The different priorities are as follows:

- Construction 1 Priority - Project scheduled to be built in the next 10 years
- Construction 2 Priority - Project scheduled to be built in 10 to 20 years
- Construction 3 Priority - Project scheduled to be built after 20 years

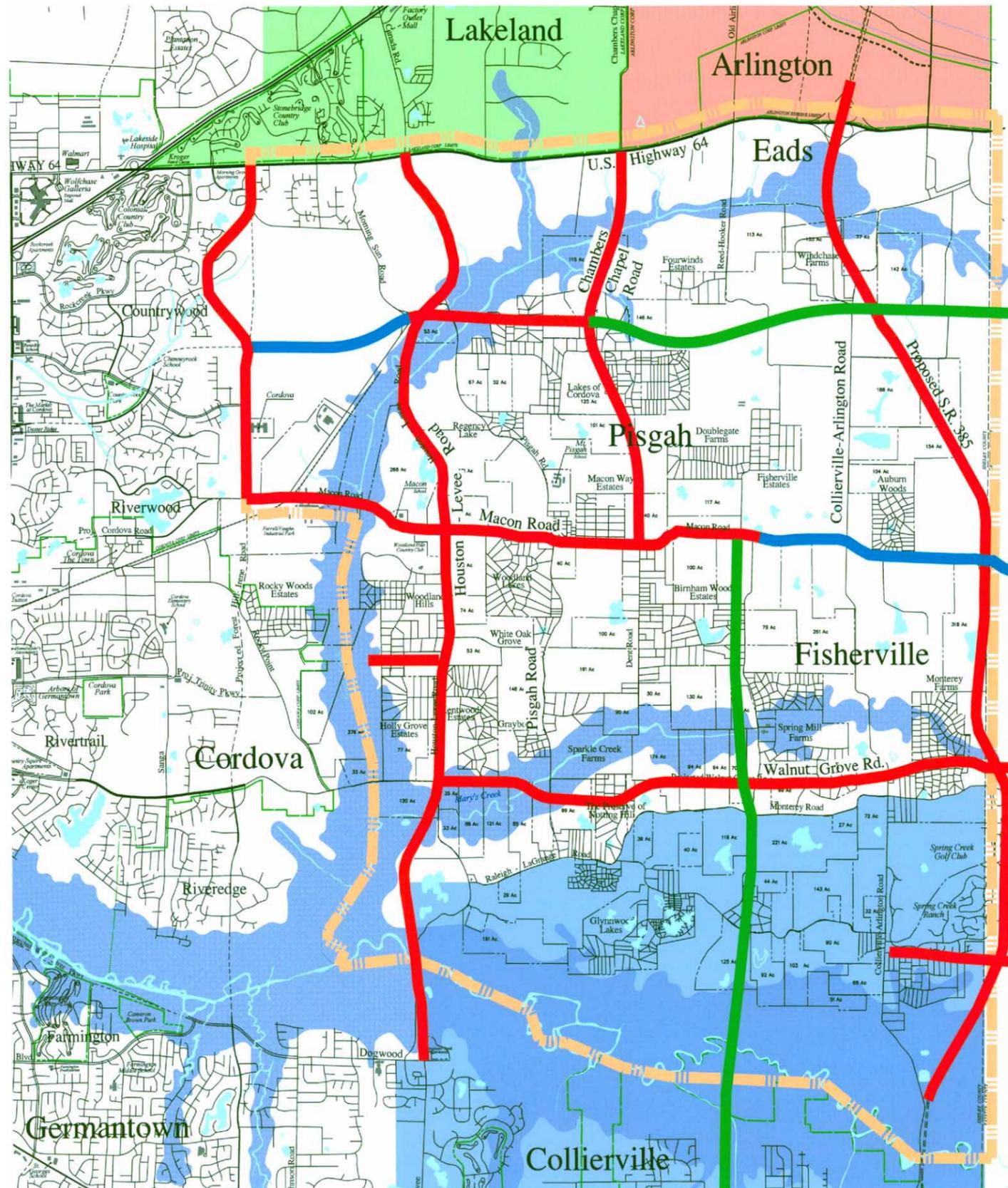
Gray's Creek Area Plan
 Memphis & Shelby County Office of Planning & Development

Memphis, Tennessee

05.27.99 Rev - Summer 2001

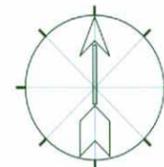
Sheet 18 of 24

© 1999 Looney Ricks Kiss Memphis Nashville Princeton



Legend

- Floodplain
- Study Area Boundary
- Construction Priority 1
- Construction Priority 2
- Construction Priority 3



Versailles

CIRCULATION PLAN

The land use pattern envisioned by the Alternative Plan will provide for more street connections and require fewer 6- and 7- lane roads than the development patterns in the Existing Trend. The differences between the Existing Trend and the Alternative Plan are numbered on the map and include:

1. The Alternate Plan proposes an extension of Chambers Chapel Road, to Walnut Grove Road, while the Existing Trend does not include this connection.
2. Walnut Grove Road is shown as 4 lanes in the Alternative Plan as opposed to 6 or 7 lanes in the Existing Trend.
3. The Alternative Plan shows Chambers Chapel Road as 4 lanes from Macon Road to Highway 64 as opposed to 6 lanes in the Existing Trend.

One general theme of the Circulation Plan is the extension of existing roads to provide connections between residential areas and to allow arterial roads to be developed to a lesser number of lanes.

An additional aspect of the Alternate Circulation Plan is the set of collector streets identified on the plan. These collectors will be 40 feet in pavement width with a 60 feet right of way and will provide alternate routes of travel to the main arterial. A collector road of this width can accommodate about 8000 vehicles per day. As a measure of expected volume, a residence generates about 10 trips per day. Traffic generated by commercial development is dependent upon the size and type of business.

This set of collector streets is required if the arterials are going to be reduced in size as indicated. Developments in area covered by the plan must be required to dedicate and improve these collector roads if they pass through or about the development. A lack of collector streets will overburden the smaller arterials as build out occurs.

Locations of interchanges on the proposed Route 385 should be reviewed in light of the Alternative Plan. Tennessee DOT is currently pursuing a design that has interchanges at U.S. 64 and Macon Road, which contradicts the Alternative Plan's recommendation that Macon Road remain as a 2-lane collector road, with the new roads on new alignment (i.e., Dexter and Walnut Grove) serving as the major roads in the long-range circulation plan.

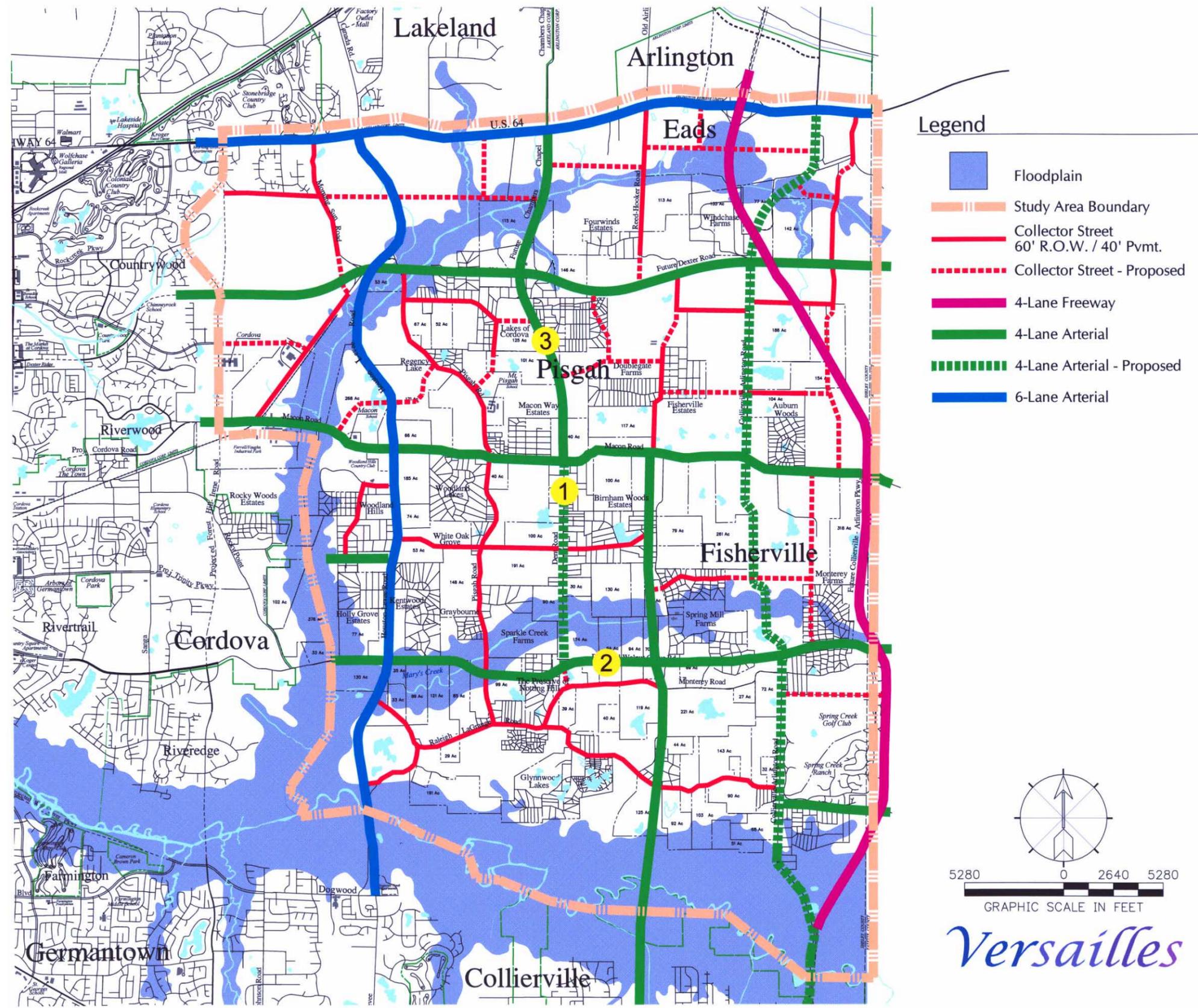
Gray's Creek Area Plan
Memphis & Shelby County Office of Planning & Development

Memphis, Tennessee

05.27.99 Rev - Summer 2001

Sheet 19 of 24

© 1999 Looney Ricks Kiss Memphis Nashville Princeton



CIRCULATION PLAN:

Recommended Road Cross Sections

The following road cross sections are recommended:

2-Lane Urban

This cross section to include curbs, gutters and sidewalks should be used in a residential development with any density greater than 1/2-acre lots.

2-Lane Rural

This roadway cross section can be used in a rural type development as defined by the development regulations. Rather than curbs, gutters and sidewalks, it features drainage swales.

4-Lane Urban

This roadway cross section can be used when the roadway may need to carry a higher volume of traffic. Median openings can be provided at intersecting streets. Medians could possibly be landscaped, but maintenance of these medians would need to be agreed upon by the governing body. Urban roads feature curbs, gutters and sidewalks.

4-Lane Rural

This roadway is similar to the 4-lane urban divided roadway and potentially could be used on roadways that may need to be widened in the future. It features drainage swales rather than curbs, gutters and sidewalks.

(See sheet 13 for typical subdivision road layouts/design.)

Corridor Overlay Zones

Corridor overlay zoning, which requires large minimum lot widths and deep building setbacks, should be applied to rural roads where it is important to maintain the existing character. Roads where these zones should be applied include Collierville-Arlington, Macon, Reed Hooker, Monterey and Pisgah, as well as any of the study area's lower-tier roads, which will remain 2-lane roads. Corridor overlay zoning will not apply at mixed-use centers. While the requirements of the overlay zoning will govern lot sizes and building setbacks, land use will continue to be controlled by the underlying base zoning.

Gray's Creek Area Plan

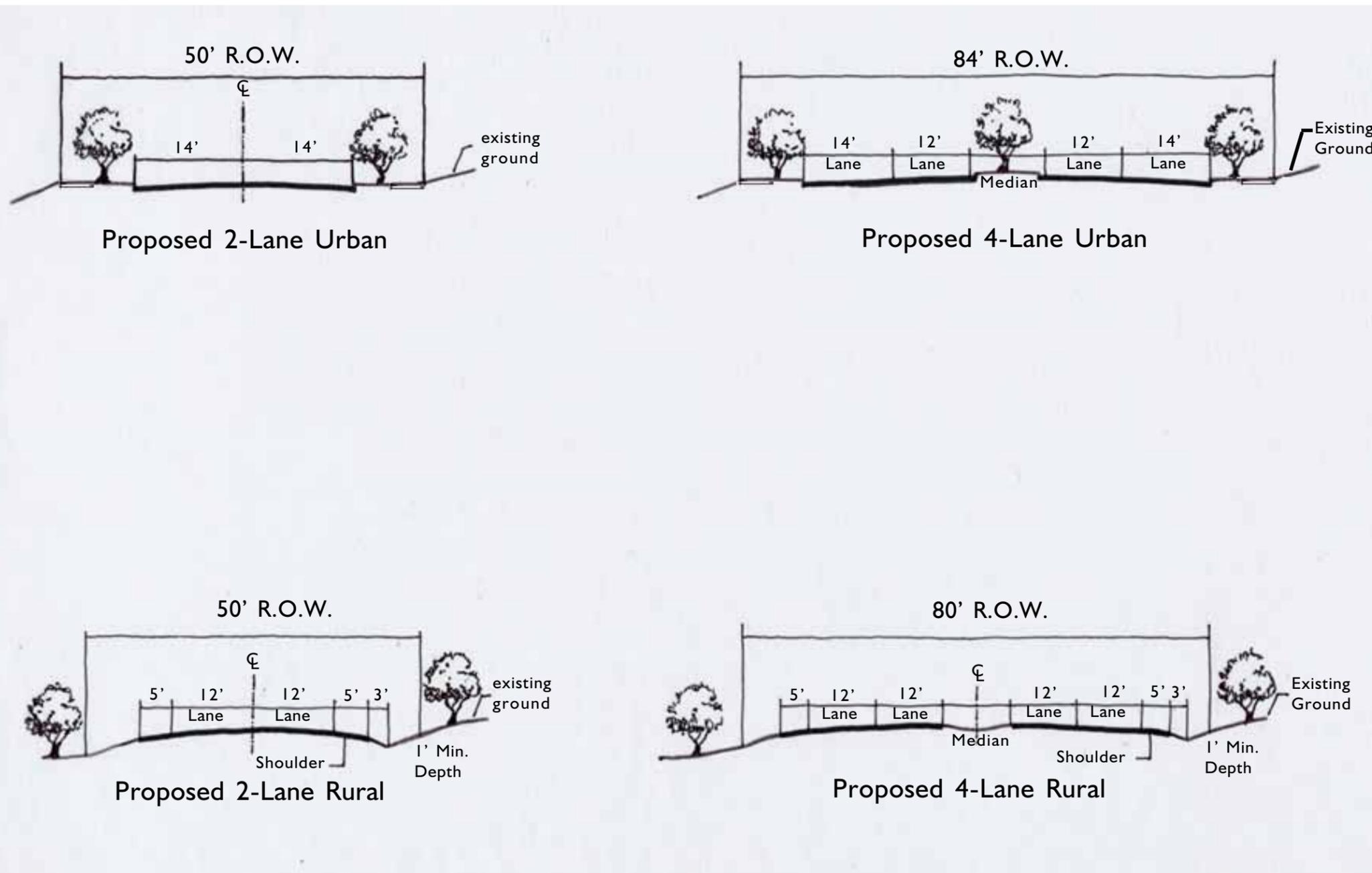
Memphis & Shelby County Office of Planning & Development

Memphis, Tennessee

05.27.99

Sheet 20 of 24

© 1999 Looney Ricks Kiss Memphis Nashville Princeton



WASTE WATER MANAGEMENT

The Memphis sanitary sewer system is predominately a gravity based network consisting of major interceptor lines and numerous outfall lines that extend to individual properties within the particular sewer basin. The lateral sanitary sewer system within the subdivision (including taps to individual lots) is the final component. The City of Memphis has been responsible for the construction of the intercept sewers. The Government, private property owners, and in some cases joint agreements between Government and private property owners have constructed the outfall lines. The private property owner constructs the lateral system within a subdivision.

The Grays Creek sanitary sewer "backbone" system (the Interceptor) is a major expansion of the sewer system infrastructure. This project was committed to in 1995 after extensive study. It was designed to collect all of the sewage from the entire Grays Creek study area, that being basically from Highway 64 on the north to the Wolf River on the south, and east from Forest Hill Irene Road to the Shelby/Fayette county line. The design anticipated a land use pattern throughout the Area requiring approximately 22,000 sewer taps by year 2020. The cost for this project is paid for by a combination of additional sewer development fees per lot and an additional sewer usage fee within the area. The expected completion date for the interceptor construction project is 2003.

Obviously, additional outfall lines must be constructed to connect upstream developments to this pipe network. The attached Sanitary Sewer Outfall Corridor Plan shows schematically how such lines would be extended to connect upstream properties to the interceptor. The usual procedure is for a developer to extend these outfall lines from the interceptor by generally following the natural drainage ways that serve the proposed development. This assures that the installed line serves as much of a given drainage basin as possible. It also sometimes requires that a line be extended through a property in order to reach properties further upstream in the basin. In some cases this intermediate property as not yet been developed.

Since Grays Creek Area is currently developing with large acreage lots (2 acre or greater) these developments, in some cases, do not face the normal subdivision review and approval procedures. It is essential that orderly extension of the sanitary sewer network be protected and provided. Therefore the following policies should be adopted:

All developments should be subject to a review and approval process by the City of Memphis during which the design and construction of sewers (dry and wet) can be required.

All developments in the Grays Creek must be required to install a lateral sewer system to service the individual lots. In a few instances two (2) acre and larger lots might be excluded

from the necessity of tying into the sewer system. Obviously, these sewer pipes may be "dry" initially until the outfalls are connected to the interceptor.

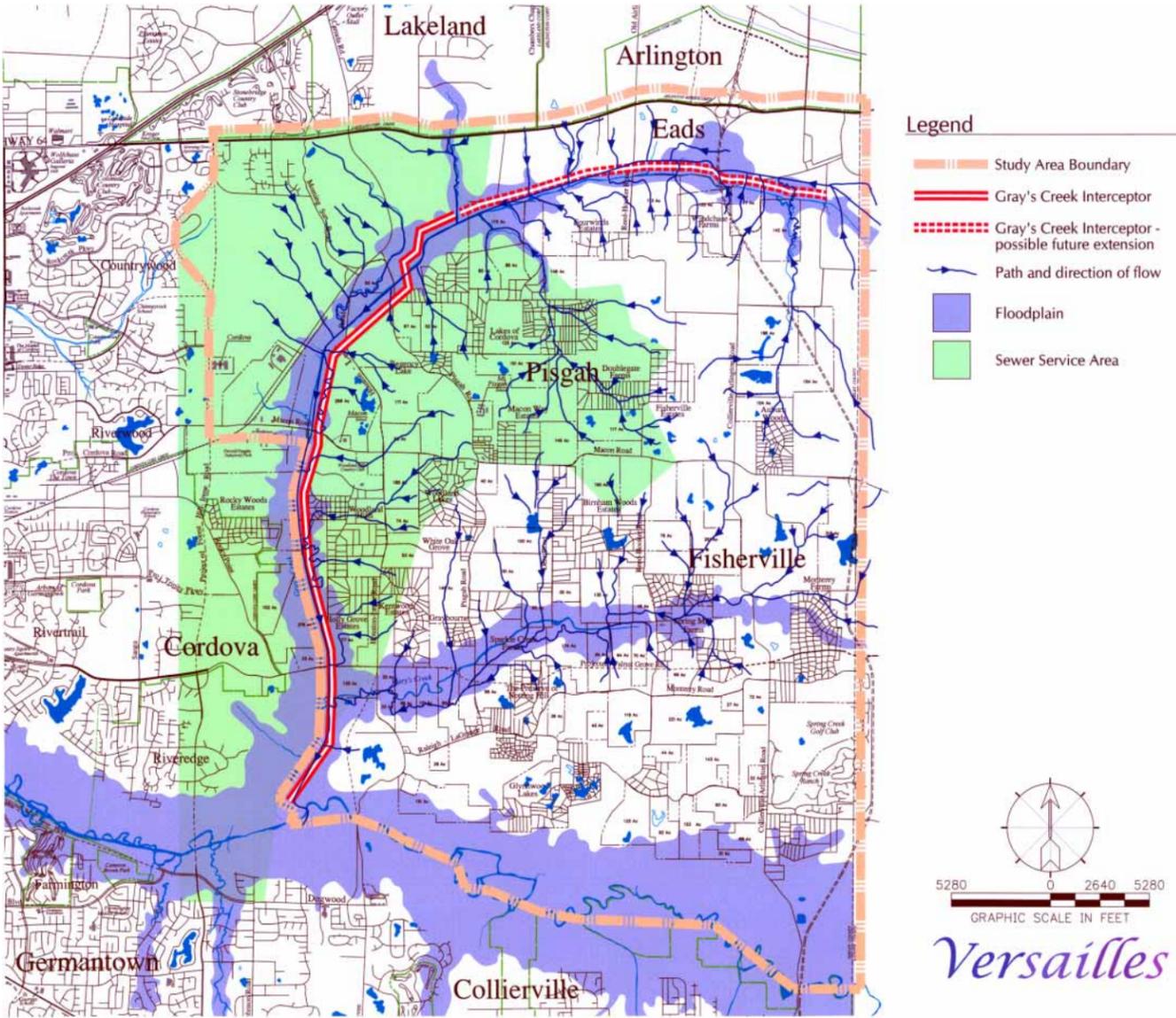
All properties must be required to provide sewer outfalls through their properties to serve upstream neighbors.

All properties within the Grays Creek basin must share in the cost of the Grays Creek Interceptor project, as defined in the Balanced Growth Policy Agreement, based on criteria that will ensure its payback regardless of the development density. This area has now been reduced by the amount of land within Collierville's Reserve Area that will not be serviced by the Grays Creek project.

The adoption of these policies will help ensure that the sanitary sewer system is properly developed in the Grays Creek Plan Area, as well as serving two ancillary purposes:

It will reduce the amount of septic tank installation in the area that is reported to have "windows" into the drinking water aquifer.

It will assure that the City of Memphis recovers its investment in the Grays Creek Interceptor



Zoning Measures

Environmental Overlay Zone

- The zoning ordinance should be amended to include an *environmental overlay zone* which applies to floodplain areas and areas suspected of being in the aquifer recharge area which do not have access to sewers. Sites in the suspected aquifer recharge zone should test based on the standards established by the Memphis Groundwater Institute. If a site is determined to be in the recharge area and does not have sewers, the overlay would be activated and densities should not exceed .5 units per acre. The overlay also would limit densities in the floodplains to .5 units per acre. Land use would still be governed by the base zoning, which is AG in most cases.

Corridor Overlay Zone

- The zoning ordinance should be amended to include a *corridor overlay zone*. The corridor overlay zone should be applied to rural roads where it is important to maintain the existing character of development. The overlay would address issues such as minimum lot widths and building setbacks which impact the character of a road.

Open Space Overlay Zone

- An *open space overlay zone* should be established and applied to areas with permitted densities of .5 units per acre or less. This overlay would allow developers to receive density bonuses if 50% or more of a site retained as open space and placed in conservation easement in perpetuity. A table in the supplementary Background Study provides the density bonuses available to developers based on the amount of a site preserved as open space.

Urban Design Overlay Districts

- The zoning ordinance should be amended to provide for *urban design overlay districts* which would be applied to the various mixed-use centers identified in the Alternative Plan. The overlay districts should be based on the commercial design principles outlined in this plan. Development plans submitted in these districts would be reviewed according to the criteria in the urban design overlay.

Special Character Zones

- Special character zones* should also be established and applied to Fisherville and Eads to insure compatible and sensitive future development. Cellular towers of a stealth design will be permitted in special character zones.

Uses in the Aquifer Recharge

- The land uses permitted with administrative site plan approval in the AG district should be carefully monitored, especially for areas located within the confirmed aquifer recharge area. Similarly, some of the uses permitted with legislative site plan review subject to a special use permit are too intensive in the aquifer recharge area. Uses currently allowed which should be reevaluated include: boarding houses, fraternity/sorority houses, group shelters, homes for the aged, dorms, airports/heliports, cemeteries/mausoleums, day care centers, day care homes, radio/TV studios, vet clinics, utility facilities, landfills, metal/sand/gravel mining and other related processing and planned developments.

Development Measures

Subdivision Reviews

- Under the current development regulations, OPD does not review the subdivision of parcels which are four or more acres and have a minimum of 50 feet of frontage on a public road. A subdivision could be developed with a street which is “stubbed out” and is intended to be extended in the future as further subdivision occurs. A developer could extend the “stubbed out” street and create a cul-de-sac, if the lots fronting the cul-de-sac were larger than 4 acres. All subdivisions should be reviewed, regardless of size, to provide for as many road connections as possible.

Maximum Block Length

- The existing regulations do not set a maximum block length. The Alternative Plan recommends that blocks be no longer than 600 feet to provide a more pedestrian-friendly environment.

Street Connections

- The language in the existing subdivision regulations

should be amended to state that “all streets shall connect unless man-made or natural features (topography, highways, or existing development) make it impossible.

Reverse Frontage Buffers

- Amend subdivision regulations to specify that any reverse frontage lots should have extensive landscaping which completely obscures their visibility on a year-round basis.

Other Measures

Grading Permits

- Permits should be required for any grading within the Gray’s Creek Area, and such grading should not be speculative but must be consistent with approved development plans. The Shelby County Engineering Department shall administer this process.

Major Street Plan Amendments

- The Major Street Plan should be amended to:
 - Designate Houston Levee as a 4-lane road
 - Designate Walnut Grove Road as 4 lanes from Houston Levee to Fayette County Line
 - Designate Macon Road as 2 lanes from Chambers Chapel Road to Hwy. 385
 - Extend Chambers Chapel Road as 2 lanes from Macon Road to Walnut Grove Road

Stormwater Management

- The recommendations in the “Metro Stormwater Study” should be followed as a means of controlling stormwater runoff in the study area.

Aquifer Recharge Delineation

- A comprehensive study to determine the boundaries of the aquifer recharge zone should be conducted by the Memphis Groundwater Institute.

IMPLEMENTATION: Public Policy Measures

Implementation of comprehensive and special area plans such as the Gray’s Creek Area Plan is primarily accomplished through public policy. For example, the implementation of recommendations for the Plan’s roads, utilities and greenways will be achieved through the ultimate design and finding allocation. Land use and development recommendations, on the other hand, should be implemented through zoning and development regulations.

It is recommended that a “super majority vote (2/3)” be retained for all rezonings, subdivisions, site plan approvals, or Planned Developments in the study area. The intent of requiring a “super majority vote” is to ensure that any approvals which are inconsistent with the Plan are carefully considered and based on sound planning in the best interest of the community.

The Steering Committee, which guided the preparation of this Plan, should assume an active role in the future development of the Gray’s Creek Area. The Steering Committee should monitor development requests in Gray’s Creek and work with OPD, business leaders, residents, and elected officials to make sure the recommendations in this Plan become a reality through public spending and policy.

The “Interim development regulations for Highway 64 and Houston Levee Road located within the Gray’s Creek Sewer Basin” is the only planning document that currently exists which applies specifically to Gray’s Creek. The interim development regulations only address the areas along Highway 64 and Houston-Levee Road.

Many of the recommendations in the interim development regulations are consistent with the proposed Alternative Plan. Both plans discourage strip commercial development and envision future commercial development occurring in a hierarchy of mixed-use centers along Houston Levee Road and Highway 64. The areas between the centers are designated as residential corridors. The Alternative Plan, however, addresses a larger area than the interim development regulations and designates four neighborhood-scale mixed-use centers in the interior of the study area.

Gray’s Creek Area Plan

Memphis & Shelby County Office of Planning & Development

Memphis, Tennessee

05.27.99

Rev - Summer 2001

Sheet 22 of 24

© 1999 Looney Ricks Kiss Memphis Nashville Princeton

Definitions

Alternative Plan: A recommended plan for Gray's Creek which attempts to manage growth and enhance the quality of life by emphasizing development patterns which respect environmental conditions, reduce reliance on the automobile, and are compatible with the area's rural character. The Alternative Plan recommends that the design of residential and commercial areas be based on traditional town planning principles as commonly practiced prior to roughly 1945.

Aquifer Recharge Area: Aquifer recharge areas are locations with soil and geological conditions that allow rainwater to be absorbed and replenish the groundwater supply. Such areas are critical to the quality and quantity of Shelby County's drinking water.

Big Box Retail: These are typically large, one-story retail buildings exceeding 100,000 sq. ft. which locate along major arterials.

Charrette: An intensive planning and design workshop in which a diverse range of stakeholders and the general citizenry participate by "brainstorming" planning ideas for an area. The result of a charrette is usually a concept plan which synthesizes the ideas of participants with base information.

Cluster Development: Clustering is intended to preserve open space by concentrating development on only a portion of a site. As implemented through overlay zoning in some communities, if 50% or more of a site is reserved as open space, a developer may receive a density bonus that would allow the development of more units than is allowable under the existing base zoning regulations. Cluster development can preserve natural features, provide for open space, and also reduce the distance that roads and sewers have to be extended, thereby reducing development costs. The permanent preservation of open space is typically ensured through either a conservation easement or the developer's dedication to the municipality.

Compact Development: Compact development occurs at relatively high densities that fully utilizes existing infrastructure and features a cohesive pattern of development. Compact development often creates opportunities to preserve common open space.

Cul-de-sac: A dead end residential street with a circular turnaround.

Density Bonus: A density bonus is an incentive offered to developers. In exchange for reserving at least 50% of a site as open space, a developer is given the option of building more units than is typically allowed under the base zoning. As a result, the

overall density on the site is increased, but more open space is provided than is normally required under the base zoning.

Existing Trend: This trend represents a low-density sprawl form of development characterized by strip commercial development, segregated land uses, isolated enclaves of residential development, and a disconnected street network.

Floor Area Ratio (FAR): The floor area ratio is used to measure the intensity level of development on a given site. It is measured by dividing the building floor area by the site area.

$$\text{The FAR} = \frac{\text{total building floor area}}{\text{total site area}}$$

Fiscal Impact Evaluation: A process which analyzes the costs and revenues associated with development. Revenues usually consist primarily of the property taxes generated by development, and costs are generally calculated by determining how much a government must spend to service new development (e.g., building new roads, educating students, etc.). Costs and revenues are compared to determine whether new development will have either a positive or negative fiscal impact. A positive fiscal impact means the government receives more money in revenues than it expends to service the new development.

Greenway: A greenway is a linear open space that usually follows the floodplains of streams and rivers. Greenways are typically improved with a paved, gravel, or dirt trail to accommodate walkers, joggers, skaters and cyclists.

Overlay Zone: An overlay zone is intended to address design issues which are not adequately addressed through the underlying base zoning. In the Alternative Plan, overlay zones are used to encourage cluster development, limit the intensity of development in the aquifer, and preserve the special character of certain areas in Gray's Creek.

Pedestrian-friendly Streetscape: A streetscape includes the street, sidewalks, and any additional public right-of-way associated with the street. A pedestrian-friendly streetscape emphasizes the pedestrian over the automobile and includes minimal street widths, wide sidewalks, street trees, pedestrian crossings, benches, and human-scaled lighting. Pedestrian-friendly streetscapes are usually framed by buildings which are designed at a human-scale and have inviting storefronts or front porches that create an interesting environment in which to walk.

Planned Unit Development District: A planned unit de-

velopment district is an alternative zoning process that allows for the development of land in a well-planned and coordinated manner, providing opportunities for more efficient utilization of land than would otherwise be permitted by conventional zoning.

Residential Density: Residential density is a measure of the number of units per acre. In this planning document's Alternative Plan, higher density refers to development at 4 or more units per acre; moderate density refers to development between 2 and 4 units per acre; and low density refers to development at less than 1 unit per acre.

Special Character Zones: This is a type of overlay zoning applied to communities such as Fisherville and Eads to preserve the existing character. The requirements of these overlay zones would supercede the requirements of the base zoning in regard to design issues. Cellular towers of a stealth design will be permitted in special character zones.

Sprawl: A low-density form of development which utilizes infrastructure inefficiently and segregates land uses, thereby leaving the automobile as the primary means of transportation.

Strip Commercial Development: Strip commercial development extends in a linear fashion along major roads. Strip commercial development emphasizes the automobile to the detriment of the pedestrian and does not typically include amenities such as sidewalks, landscaping, street trees and other features which encourage people to walk. There are typically no pedestrian connections between buildings and people have to get in their car to go from one destination to another. Buildings typically do not address the street and frequently have large surface parking lots in front. Car dealerships, fast food restaurants, convenience marts, and gas station are examples of predominant uses in commercial strips.

DEFINITIONS

These definitions are intended to assist readers in understanding concepts that are used throughout this document.

Gray's Creek Area Plan

Memphis & Shelby County Office of Planning & Development

Memphis, Tennessee

05.27.99

Rev - Summer 2001

© 1999 Looney Ricks Kiss Memphis Nashville Princeton

These recommendations are pursuant to the findings of the Gray's Creek Ad Hoc Committee:

1. Establish Residential Corridors as defined within this study which disallows any non-residential development with the exception of intersections as delineated on Page 8.
2. Create additional zoning classifications for residential estate lots pursuant to the recommendations on Page 11.
3. There has been considerable discussion regarding the location of aquifer recharge areas in the region. As a result, the ad hoc committee suggests that further research and testing be conducted in order to locate and define aquifer recharge areas within the Gray's Creek study area.
4. Adopt the Circulation Plan as a long-term capital improvement strategy for major and collector roads as indicated on Page 19.
5. Encourage the use of boulevard designs where appropriate.

**RECOMMENDED FUTURE
ACTIONS OF THE MEMPHIS
CITY COUNCIL AND THE
SHELBY COUNTY
COMMISSION**

Gray's Creek Area Plan

Memphis & Shelby County Office of Planning & Development

Memphis, Tennessee

05.27.99

Rev - Summer 2001

Sheet 24 of 24

© 1999 Looney Ricks Kiss Memphis Nashville Princeton